



Headline House, Ashford -17/00782/FUL.

Scale 1:1,250

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Planning Committee

20 September 2017



Application No.	17/00782/FUL
Site Address	Headline House, Stanwell Road, Ashford TW15 3HQ
Applicant	Mr Mike Daly
Proposal	Erection of a two storey building with second floor accommodation to provide 5 no. one bedroom flats and 5 no. two bedroom flats with associated parking and amenity space following demolition of the existing commercial building on site.
Ward	Ashford Town
Called-in	N/A
Case Officer	Siri Thafvelin

Application Dates	Valid: 27/07/2017	Expiry: 26/10/2017	Target: Under 13 weeks
Executive Summary	<p>The application site comprises a broadly rectangular plot of land located on the eastern side of Stanwell Road, on the junction with Chaucer Road. The site is currently occupied by a single storey commercial building with a mezzanine floor. The current structure covers the majority of the site with a small grass area on the Stanwell Road frontage and some off street parking/access on the Chaucer Road frontage. The wider area is characterised by two storey residential dwellings, with a mix of both detached and semi-detached. On the opposite side of the street to the west is St Hildas Church, a Grade II listed building.</p> <p>The proposal involves the demolition of the existing building on site and the construction of a single building which would be two storey in appearance with second floor accommodation within the roofspace. The building would have staggered frontage onto Stanwell Road and would 'turn the corner' into Chaucer Road. The flats would be served by a car parking area to the rear accessed from Chaucer Road, and a communal garden area.</p> <p>The proposal is similar to the previously approved scheme for 9 no. flats approved earlier this year (17/00099/FUL) and the principle of demolishing the existing commercial building and creating a new residential development continues to be acceptable. It is considered that the design and appearance would have an acceptable impact on the character of the area and that it would not harm the setting of the adjacent Grade II listed building. The proposal would continue to have an acceptable relationship with neighbouring properties and would have an</p>		

	appropriate impact on their amenities. The proposal would provide 14 off-street car parking spaces which meets the Council's minimum car parking standards and is considered acceptable.
Recommended Decision	Approve the application subject to conditions as set out at Paragraph 8 of the Report.

MAIN REPORT

1. Development Plan

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

- EN1 – Design of New Development
- EN5 – Buildings of Architectural and Historic Interest
- HO4 – Housing Size and Type
- HO5 – Density of Housing Development
- CC3 – Parking Provision
- CC1 – Renewable Energy

2. Relevant Planning History

2.1 The current structure on site was constructed in the late 1980s and prior to that the site was used as a garage. In recent years an application to redevelop the site for residential use was refused on design and layout grounds. Two applications for residential development on the site have since been granted planning permission. The relevant applications are listed below:

17/00099/FUL	Erection of a two storey building with second floor accommodation to provide 2 no. one bedroom flats and 7 no. two bedroom flats with associated parking and amenity space following demolition of the existing commercial building on site	Grant Conditional 13.03.2017
15/01513/FUL	Erection of two storey building to provide 1 no. one bed flat, 6 no. two bed flats and 1 no. three bed flat with associated parking and amenity space following demolition of the existing commercial building on site.	Grant Conditional 18.01.2016
14/01836/FUL	Erection of 2 no. two storey blocks containing a total of 4 no. one bed flats and 4 no. two bed flats (2 no. one bed and 2 no. two bed in each block) with associated parking and amenity space following demolition of existing industrial unit on site.	Refused 20.05.2015

SPW/FUL/87/5 Conversion of existing garage building to provide 422.4 sq m (4,547 sq ft) of office accommodation (involving the provision of a mezzanine floor in part), elevation alterations, alterations to Chaucer Road vehicular access and provision of 16 car parking spaces. Grant Conditional 11.03.1987

3. Description of Current Proposal

- 3.1 The application site comprises a broadly rectangular plot of land located on the eastern side of Stanwell Road, on the junction with Chaucer Road. The site is currently occupied by a single storey commercial building with a mezzanine floor. The current structure covers the majority of the site with a small grass area on the Stanwell Road frontage and some off street parking-access on the Chaucer Road frontage. The wider area is characterised by two storey residential dwellings, with a mix of both detached and semi-detached. On the opposite side of the street to the west is St Hildas Church, a Grade II listed building.
- 3.2 The proposal would involve the demolition of the existing building (which has a footprint of 585m²) on site and the erection of a two storey building with habitable accommodation within the roofspace. The building would have a broadly L-shaped footprint with a width of 21.9m along Stanwell Road and 17.1m along Chaucer Road. The building would have a pitched roof with a central flat roof section and gable ends in the east, west and south elevations. The maximum height of the main roof and ridge height of the gables would be 8.9m and the height to the eaves would be 5.9m.
- 3.3 The proposed building would comprise 1 no. one bedroom 2 no. two bedroom units unit at ground floor, 1 no. one bedroom unit and 3 no. two bedroom units at first floor, and 3 no. one bedroom units at second floor. The main access to the building would be from Chaucer Road and there would be a rear entrance in the east elevation with access from the parking and amenity area.
- 3.4 The building would be served by a parking area to the rear (east) of the building with a total of 14 spaces, including one disabled bay with access from Chaucer Road. A communal amenity area would also be located to the rear of the building and the front and sides of the site would be landscaped. An existing tree on the side of the Stanwell Road frontage would be retained. The building would incorporate a bin store, and an integral cycle store with space for ten bicycles.
- 3.5 Directly opposite the site to the west lies St Hildas Church which is a Grade II listed building. This church was completed in 1928 and is a prominent landmark within the area. Its design use of red brick is distinctive and the proposal has reflected elements of the design and incorporated red brick.

4. Consultations

- 4.1 The following table shows those bodies consulted and their response.

Consultee	Comment
County Highway Authority	No objection subject to imposition of conditions

Thames Water	No objection. Has made comments in relation to surface water drainage which are to be attached to the decision notice as an informative.
Surrey Police	No objection. Requests a condition that requires the development to achieve the full Secured by Design award.
The Council's Tree Officer	Any comments will be reported orally at the meeting.
The Council's Heritage Officer	No objection
The Council's Head of Streetscene	No objection
Environmental Health – Pollution Control Officer (Contaminated Land)	No objection subject to imposition of conditions
Environmental Sustainability	No objection
SCAN	Objection

5. Public Consultation

5.1 39 neighbour notification letters were sent. A site notice was displayed and a notice was published in the local press. 24 responses from 21 residences have been received to date, including comments from SCAN. Issues raised include:

- Out of character
- Height of proposed development
- Lack of parking
- Parking is below minimum standards (*Officer note: Amended plans have been submitted that increase the number of parking spaces from 13 to 14 to meet the Council's minimum parking standards.*)
- Impact on local business and customer parking
- Overdevelopment
- Overlooking
- Noise
- Rubbish and flytipping at future development
- Wheelchair access

6. Planning Issues

- Design, appearance and visual impact
- Residential amenity
- Housing size, type and density
- Parking
- Impact on a listed building
- Impact on existing trees

7. Planning Considerations

Comments on the Previous Application

- 7.1 The most recent application (reference 17/00099/FUL) was granted planning permission on 13 March 2017. The scheme was very similar to the current proposal in terms of design but comprised 2 no. one bed flats and 7 no. two bed flats with associated parking and amenity space to the rear. The layout of the current proposal is very similar to the approved scheme; the design of the building has not changed externally and the overall height, gable features facing Stanwell Road and Chaucer Road, the fenestration and use of private balconies, remain the same.
- 7.2 The main difference between the two schemes is the internal layout of the second floor and mix of flat sizes. There have also been some changes to the site layout to create an additional parking space and increased private amenity space for two of the ground floor units.
- The most recently approved scheme was for:
1. 2 no. one bed and 7 no. two bed flats with 13 parking spaces
- The proposed scheme is for:
2. 5 no. one bed flats and 5 no. two bed flats with 14 parking spaces
- 7.3 Compared with the last approved scheme, the layout, footprint, massing, height and location of windows have not changed with the current revised scheme. The external changes relate to an additional parking space, insertion of two privacy screens on the northern elevation and an increase in the size of two patios.
- 7.4 Like the previous proposal, the scheme under consideration has been designed to pick up on features of the adjacent listed building whilst maintaining its own distinct identity. It is considered that the staggered gables, balconies and choice of materials would not cause harm to the setting of the listed building and that it would represent an improvement over the existing industrial looking building on site.

Design, Appearance and Visual Impact

- 7.5 The building has been designed to address both the Stanwell Road and Chaucer Road frontages. The Stanwell Road frontage would include staggered gable ends with glass balconies at first floor level. The gables would be staggered in such a way that on the corner of Stanwell Road and Chaucer Road the gable would be set back from the junction and would allow the building to 'turn the corner' into Chaucer Road. The Chaucer Road frontage would again feature a gable end to help link the whole building together. The overall design, form and choice of materials would give the building a contemporary feel, albeit with a pitched roof and strong gable features. The building would have an area of flat roof, but this would be mostly obscured by the pitched roofs and it is not considered it would be viewed from within any street aspect as a flat roof structure.
- 7.6 The proposed building would have a smaller footprint than the existing building. The existing building has a shallow pitch over with a maximum height of 7.31m. The proposed building would have a maximum height of 8.9m. Whilst this is clearly higher than the existing, due to the significantly reduced footprint it is considered the increase in height would not have an unacceptable impact on the streetscene. The height of the building would not appear out of context within both the Stanwell Road and Chaucer Road frontages and would not 'compete' with the adjacent church in terms of scale. As set out in paragraph

7.3 the height of the current proposal is the same as the previous proposal and it is considered the scale of the building when viewed in relation to the whole site and its parking, amenity and landscaped areas would be acceptable.

- 7.7 The proposal would respect the building line within both Stanwell Road and Chaucer Road. The element closest to no. 104 Stanwell Road (the adjoining detached dwelling to the north) would be in line with the frontage of that property before stepping out slightly to the first of three staggered gable frontages. The northernmost gable would be set forward of the prevailing building line and the other two gables would be set in line with no. 104 Stanwell Road and then further back, respectively. The corner closest to no. 106 Stanwell Road (the dwelling to the south on the other side of Chaucer Road) would be set behind the front elevation of this building. The gable feature on Chaucer Road would be slightly forward of the prevailing building line but the majority of this frontage would be broadly in line with the properties on the northern side of the street.
- 7.8 It is therefore considered that the proposal would be of a layout and form that would respect the character of the area and that it would successfully address the prominent corner plot. The building would be of a comparable height to the adjoining dwellings and its contemporary design and choice of materials including red brick would help to reflect the red brick of the adjacent church, whilst the more modern materials such as the glass balustrades would help the building maintain its own identity.
- 7.9 Like the previously approved scheme the proposal would include habitable accommodation within the roof. In order to provide sufficient light to these rooms a number of rooflights have been included. The rooflights have been positioned symmetrically and it is considered that the design of the roof would not appear out of scale with the host building. The proposal would include an area of flat roof and it is considered that this would not be so visually prominent as to cause harm to the wider area. It should be noted that the existing building has two parapet roof edges on both flanks which give the impression of a flat roof and that the adjoining church hall building has a flat roof section. It is therefore considered that the flat roof element would be an acceptable design form in this location.
- 7.10 The proposed shared amenity area would be located in the north east corner of the site and would be relatively well screened from the surrounding area so as not to feel unacceptably overlooked. The proposal would include a landscaped frontage along both Stanwell Road and Chaucer Road and would retain an existing tree located at the front of the site.
- 7.11 The proposed parking area would be located to the rear (east) of the proposed building. It would include a total of 14 spaces, one of which would be a disabled parking space. It is considered its position to the rear of the building would be acceptable and that it would not lead to the parking area having a dominant appearance on the site. The car park would be partially screened by the building and landscaped planting, which can be controlled by a condition.

Residential Amenity

- 7.12 The proposed building would have an acceptable impact upon the amenities of the adjoining neighbouring properties. The scheme would not have an overbearing impact and would not result in an unacceptable loss of light to the adjoining properties. The nearest existing residential property to the proposed

building is no. 104 Stanwell Road which is a detached property to the north of the site. Whilst the existing building, Headline House, is lower, it is located directly on the boundary of the site, whereas the proposed building would be set in from the boundary by 1.3m. There is also an access road which runs between the application site and no. 104 which serves a trio of garages. The 45 and 25 degree lines are used as a guide to ensure that new development does not block light reaching the windows serving habitable rooms and the proposal would not infringe either a vertical or horizontal 45 degree line when measured from the closest windows serving habitable rooms of the adjoining dwellings. The 25 degree line would not be infringed from any fronts or rears of adjoining dwellings.

- 7.13 Along Stanwell Road the proposed ground and first floors would have full height windows with private patios at ground floor and balconies at first floor. These windows would allow the maximum amount of natural light into habitable rooms and provide an outlook to the adjacent church. There would also be full height windows at ground and first floor level along Chaucer Road. To ensure no loss of privacy to no. 106 Stanwell Road, situated on the south side of Chaucer Road, the two clear glazed first floor windows in this elevation have been positioned to avoid views into the rear garden of the neighbouring property. One second floor window is proposed in the gable, however this will be obscure glazed. There would be no clear glazed window openings on the northern flank elevation closest to no. 104 Stanwell Road that could give rise to unacceptable overlooking. The rear elevation would feature ground and first floor full height windows and the first floor units would also be served by balconies which would overlook the shared amenity area. It is considered that these rear facing windows and balconies would not lead to an unacceptable loss of privacy to the adjoining dwellings due to their separation. The Council's Supplementary Planning Document on the Design of Residential Extensions and New Residential Development (2011) sets out minimum separation distances. The proposal would have a separation distance of over 17m which exceeds the minimum of 13.5m set out in the SPD.
- 7.14 Objections have been raised by some local residents to the positioning of balconies and their impact on the privacy of the neighbouring properties. The proposed balconies would be situated a minimum of 16.5m from the nearest property in the direction they face and privacy screens would be required to prevent overlooking sideways which is considered acceptable. Furthermore, balconies were approved in these locations in the previous scheme and were considered acceptable and the current balconies will have the same impact. It is therefore considered that the impact is acceptable and that refusal can be justified on these grounds.
- 7.15 It is considered that due to their angle, the proposed rooflights would not constitute a third story in terms of assessing overlooking impact, and that they would not lead to unacceptable overlooking due to their skyward orientation.
- 7.16 With regards to the amenity of the future occupiers of the proposed ten flats, it is considered that the proposal would provide an acceptable level of residential amenity. The Department for Communities and Local Government's Technical housing standards (March 2015) sets out minimum floor areas for new units which each unit would comply with or exceed. The flats would be well provisioned with windows to allow natural light and rooms within the roofspace would be served by rooflights.

- 7.17 The scheme would require a minimum of 225m² of amenity area to fulfil the minimum amenity standards set out in the SPD. The proposal would exceed this by providing a communal garden, 5 no. balconies and 4 no. patios for a total of 229.3 m² of amenity space.

Housing Size, Type and Density

- 7.18 All the ten proposed units would be 1 or 2 bed units and so would comply with Policy HO4 which requires new development of over four units to provide at least 80% of new units as one or two bedroom units.
- 7.19 The proposed site has an area of 0.094ha and would have a density of 106 dwellings per hectare. The high density is a result of the proposed units being 1 and 2 bed flats with shared amenity space (compared for example to larger dwellings with private gardens). Whilst this is above the recommended range of 35 to 55 dwellings per hectare, Policy HO5 states that higher density development may be acceptable where it is demonstrated that the development complies with Policy EN1 on design, particularly in terms of its compatibility with the character of the area and is in a location that is accessible by non car-based modes of travel. The development complies with Policy EN1 (as explained above) and is also in a location accessible by non-car based travel, for example Ashford railway station is situated only 400m from the development (approx. 5 minutes away on foot) and the nearest bus stops are approx. 110m (approx. 2 minutes on foot) on Woodthorpe Road.

Parking

- 7.20 The proposal would provide a total of 14 off street parking spaces which would be located to the rear of the building. This would comply with the minimum parking requirements for a development of this size as required by the Council's Parking Standards Supplementary Planning Guidance (1.25 spaces per one bed unit and 1.5 spaces per two bed unit: 1.25 x 5 + 1.5 x 5 = 13.75). The proposal would provide a total of 10 secure cycle parking spaces within the main building which complies with the required one cycle space per flat.

Impact on the Adjacent Listed Building

- 7.21 Section 66 of the Listed Building Act 1990 requires a planning authority to have 'special regard' to the desirability of preserving a listed building and its setting. The application site is opposite the Grade II listed building St Hilda's Church which is a red brick building that was completed in 1928 featuring high gable ends. Any proposal for new development should demonstrate that it would not harm the building or its setting. The proposed building has been designed with staggered gable frontages to add both visual interest to the building and to reflect the large gable end of this part of the listed church facing the application site and in ancillary elements of that building. The use of strong gable features and of red brick within parts of the proposed frontage would pay regard to those design features and materials within the church, and the modern materials such as the glass balustrades would contrast and allow the new building to maintain its own identity. The National Planning Policy Framework (the NPPF) states that any harm caused to a Grade II listed building should only occur in exceptional circumstances. It is not considered that the proposal would cause harm to the listed building but rather that the proposal would serve to significantly improve the setting of the listed building by the removal of the 1980s factory building with no comparable design merit.

and replacement with a sympathetic design which would enhance the setting of St Hildas Church.

- 7.22 The Council's Heritage and Conservation Area Officer has raised no objections to the proposal and stated that: *I previously supported the approved scheme as being appropriate to the corner site and as not being harmful to the setting of the nearby listed church building. The current scheme is similar to the approved one but has now managed to accommodate an extra apartment in the roof space. This is good use of available space and does not prejudice the acceptability of the recently approved scheme.* It is therefore considered that the proposal will have no detrimental effects on the setting of the nearby Grade II listed church.

Impact on Existing Trees

- 7.23 The Council's Tree Officer has been consulted and any comments will be reported orally at the committee meeting. However, it is worth noting that the tree officer raised no objections to the previous proposal and its impact upon the existing trees on the site, subject to the imposition of a condition to ensure that the trees are not damaged during construction. The proposed planting can be agreed by the imposition of a suitable planning condition.

Need for Housing

- 7.24 When considering planning applications for housing local planning authorities should have regard to the government's requirement that they boost significantly the supply of housing and meet the full objectively assessed need for market and affordable housing in their housing area so far as is consistent with policies set out in the National Planning Policy Framework (NPPF) para 47.
- 7.25 The Government also requires housing applications to be considered in the context of the presumption of sustainable development. Relevant policies for the supply of housing cannot be considered up-to-date if the local planning authority cannot demonstrate a five-year supply of deliverable site (para 49 of the NPPF).
- 7.26 The Council has embarked on a review of its Local Plan and accepts that the housing target in its Core Strategy and Policies DPD (February 2009) of 166 dwellings per annum is significantly short of its latest objectively assessed need of 552-757 dwellings per annum (para 10.42, Strategic Housing Market Assessment, Runnymede and Spelthorne (November 2015)). On the basis of its objectively assessed housing need the Council is unable to demonstrate a five-year supply of deliverable sites.
- 7.27 Para 14 of the NPPF stresses the presumption in favour of sustainable development and that proposals which accord with a development plan should be approved without delay. When the development plan is absent, silent or relevant policies are out of date, permission should be granted unless *'any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against policies in the Framework taken as a whole or specific policies in this Framework indicate development should be restricted.'* This application must be considered having regard to the above requirements of Para 14 of the NPPF.
- 7.28 Having regard to the proposed development and taking into account the above and adopted policy HO1 which encourages new development, it is

considered that particular weight should be given to the merits of this development. There are no significant adverse impacts from this proposal and given that it will meet our acknowledged housing shortfall, it is considered that planning permission should be granted.

Local Finance Considerations

- 7.29 Under S155 of the Housing and Planning Act 2016, Local Planning Authorities are now required to ensure that potential financial benefits of certain development proposals are made public when a Local Planning Authority is considering whether or not to grant planning permission for planning applications which are being determined by the Council's Planning Committee. A financial benefit must be recorded regardless of whether it is material to the Local Planning Authority's decision on a planning application, but planning officers are required to indicate their opinion as to whether the benefit is material to the application or not.
- 7.30 In consideration of S155 of the Housing and Planning Act 2016, the proposal is a CIL chargeable development and will generate approximately £8301 in CIL Payments. This will be at a rate of £140 per square metre of new floorspace. This is a material consideration in the determination of this planning application. The proposal will also generate a New Homes Bonus and Council Tax payments which are not material considerations in the determination of this proposal.

Other Matters

- 7.31 With regard to the Crime Prevention Officer's comments, I do not consider it is appropriate to impose a condition, as requested, relating to "Secured by Design". Many of the requirements are very detailed (e.g. types of laminated glazing and window locks), elements which are not normally covered and enforced under the planning regulations. Conditions are to be imposed requiring an external lighting scheme to be implemented, partly for security purposes, and that a secure and satisfactory cycle parking area is provided. However, a copy of the officer's response has been forwarded to the applicant and it is proposed to add a relevant informative to the decision notice (see below).
- 7.32 The County Highway Authority has raised no objection to the proposal on highway grounds and has requested that relevant conditions and informatives are attached to the decision notice to ensure that the proposal has an acceptable impact on highway safety.
- 7.33 A renewable energy statement was submitted with the application and the Council's Environmental Sustainability officer is satisfied that the renewable energy requirement will be met. It is recommended that a condition is imposed to ensure that no development shall commence until details have been submitted demonstrating how 10% of the energy requirements generated by the development will be achieved utilising renewable energy methods to meet the Council's requirements.
- 7.34 An objection has been received from SCAN that the units have not been designed to provide access and facilities for wheelchair users. The proposed parking involves one disabled parking space and the applicant has advised that accessible approaches and level thresholds have been incorporated into the new building. In addition, external and internal circulation, including minimum door, corridor and stair dimensions, together with cloakroom

requirements are all in compliance with Part M of the Approved Building Regulations. It is also noted that under the Equalities Act 2010, which is separate legislation from planning, a duty of responsibility is placed on the owner of the premises to provide facilities for disabled persons to access a building. In addition, the building will be subject to building control regulation. It is recommended that an informative is to be attached to the decision notice advising the applicant of the duty to make adjustments to the property under the Equalities Act to make it accessible to disabled people.

- 7.35 Third party representations have also raised concern about the impact of the proposal on noise. The proposed building would have to meet building regulation requirements in terms of noise insulation and it is not considered that a residential development of this size would generate unacceptable levels of noise sufficient to justify refusal.
- 7.36 The proposal is not substantially different to the most recently approved scheme for this site (reference 17/00099/FUL). It is considered that it would have an acceptable impact on the character of the area and the adjacent Grade II listed building. The proposal would provide sufficient on-site car parking to meet the Council's standards and the site's location close to Ashford town centre would be considered sustainable and encourage non-car based travel.
- 7.37 Accordingly the application is recommended for approval.

8. Recommendation

- 8.1 The application is recommended for approval subject to the following conditions:
1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:- This condition is required by Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
 2. The development hereby permitted shall be carried out in accordance with the following approved plans: L2436/LP Revision A; L2436/20; L2436/21; L2436/22 received 11 May 2017, and L2436/23 Revision B and L2436/24 Revision B received 4 September 2017.

Reason:- For the avoidance of doubt and in the interest of proper planning.
 3. Before any work on the development hereby permitted is first commenced details of the materials and detailing to be used for the external surfaces of the building(s) and surface material for parking areas be submitted to and approved by the Local Planning Authority.

Reason:- To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.
 4. No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a plan indicating the

positions, design, materials and type of boundary treatment to be erected. The boundary treatment shall be completed before the building is occupied. Development shall be carried out in accordance with the approved details and maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

5. No development shall take place until full details of both soft and hard landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. The trees and shrubs shall be planted on the site within a period of 12 months from the date on which development hereby permitted is first commenced, or such longer period as may be approved by the Local Planning Authority, and that the planting so provided shall be maintained as approved for a period of 5 years, such maintenance to include the replacement in the current or next planting season whichever is the sooner, of any trees or shrubs that may die, are removed or become seriously damaged or diseased, with others of similar size and species, unless the Local Planning Authority gives written permission to any variation.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development. In accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

6. The measures set out in the Arboricultural report (ref: APA/AP/2017/091) be adhered to at all times, and any variation be submitted to and agreed in writing by the Local Planning Authority

Reason:- To prevent damage to the trees in the interest of the visual amenities of the area, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

7. Before any development commences, details including a technical specification of all proposed external lighting shall be submitted to and approved in writing by the Local Planning Authority. The external lighting on the site shall at all times accord with the approved details.

Reason:- To safeguard the amenity of neighbouring residential properties and in the interest of security.

8. No demolition, site clearance or building operations shall commence until protective fencing consisting of weld mesh panels on a scaffold framework as shown in Figure 2 of BS5837 2012 has been erected around each tree or tree group to be retained on the site in accordance with details to be submitted to and approved by the Local Planning Authority before any work on the development hereby permitted is first commenced, such details to include trenches, pipe runs for services and drains. Such fencing shall be maintained during the course of the development and no storage of materials or erection of buildings shall take place within the fenced area.

(b) The destruction by burning of materials shall not take place within 6 m (19 ft 8 ins) of the canopy of any tree or tree group to be retained on the site or on land adjoining.

Reason:- To prevent damage to the trees in the interest of the visual amenities of the area, in accordance with policies SP6 and En1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

9. Prior to the occupation of the development hereby permitted the ground and first floor kitchen/dining windows in the most northern side elevation and the second floor kitchen window in the most southern Chaucer Road elevations shall be obscure glazed and be non-opening to a minimum height of 1.7m above internal floor level in accordance with details/samples of the type of glazing pattern to be submitted to and approved in writing by the Local Planning Authority. These windows shall thereafter be permanently retained as installed.

Reason:- To safeguard the privacy of the adjoining properties in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

10. Notwithstanding the details shown on the submitted drawings, prior to the occupation of the dwellings privacy screens shall be installed on the northern side of the east-facing and west-facing balconies of the most northern first floor unit, and the eastern side of the north-facing balcony of the most eastern first floor unit in accordance with details to be submitted and approved in writing by the Local Planning Authority. These details shall provide privacy screens measuring at least 1.8m in height which shall be installed prior to the first use of the balconies and thereafter retained.

Reason:- To safeguard the amenity of neighbouring residential properties.

11. Prior to the occupation of the building the enclosed cycle storage facility shall be provided on the site in accordance with the approved plans and thereafter shall be maintained as approved.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

12. Notwithstanding the submitted plans the development hereby approved shall not be first occupied unless and until the existing vehicular access to Chaucer Road has been modified and provided with tactile paving in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority, all to be permanently retained.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 (highway safety) and CC3 (parking) of

Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

13. No new development shall be occupied until three car parking spaces have been provided with electric vehicle trickle charging points in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. The trickle charging points shall be used and retained exclusively for their designated purpose.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC3 (parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

14. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking areas shall be retained and maintained for their designated purposes.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 (highway Safety) and CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

15. The development hereby approved shall not be first occupied unless and until the existing vehicular access to Chaucer Road has been modified in accordance with the approved plan, and redundant sections of the existing access have been reinstated to kerb and footway, all to be permanently retained.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 (highway Safety) and CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

16. No development shall commence until a Construction Transport Management Plan has been submitted to and approved in writing by the Local Planning Authority, containing details of:

- a) Parking for vehicles of site personnel, operatives and visitors
- b) Loading and unloading of plant and materials
- c) Storage of plant and materials
- d) Provision of boundary hoarding behind any visibility zones
- e) Measures to prevent the deposit of materials on the highway

The Construction of the development shall be implemented in accordance with the agreed Construction Transport Management Plan.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy

Framework 2012 and policy CC2 (highway safety) and CC3 (parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

17. The proposed hardstanding area shown on the submitted plan shall be constructed to be permeable, or be designed to run off to a permeable surface within the site, in accordance with details to be submitted to, and approved in writing by the Local Planning Authority before the commencement of the development. The hardstanding area shall be completed and maintained in accordance with the approved details.

Reason:- To minimise the risk of flooding from surface water runoff.

18. No development shall commence until a report has been submitted to and agreed by the Local Planning Authority which includes details and drawings demonstrating how 10% of the energy requirements generated by the development as a whole will be achieved utilising renewable energy methods and showing in detail the estimated sizing of each of the contributing technologies to the overall percentage. The detailed report shall identify how renewable energy, passive energy and efficiency measures will be generated and utilised for each of the proposed buildings to meet collectively the requirement for the scheme. The agreed measures shall be implemented with the construction of each building and thereafter retained and maintained to the satisfaction of the Local Planning Authority unless otherwise agreed in writing.

Reason:- To ensure that the development is sustainable and complies with policies SP7 and CC1 of the Spelthorne Core Strategy and Policies Development Plan Document February 2009.

19. That within 3 months of the commencement of any part of the development period, or any such longer period as may be approved by the Local Planning Authority, facilities shall be provided within the curtilage of the site for the storage of refuse and waste materials in accordance with the details hereby approved, and thereafter shall be maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

20. No development shall take place until:

- (i) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site has been submitted to and approved in writing by the Local Planning Authority.
- (ii) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.

- (iii) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without express written agreement of the Local Planning Authority.

Reason:- To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

NOTE

The requirements of the above condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected by Contamination: Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at www.spelthorne.gov.uk.

- 21. Prior to the first use or occupation of the development, and on completion of the agreed contamination remediation works, a validation report that demonstrates the effectiveness of the remediation carried out shall be submitted to and agreed in writing by the Local Planning Authority.

Reason:- To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

NOTE

The requirements of the above condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected by Contamination: Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at www.spelthorne.gov.uk.

INFORMATIVES TO APPLICANT

- 1. The permission hereby granted shall not be construed as authority to carry out works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a licence must be obtained from the Highway Authority Local Transportation Service before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. The applicant is also advised that Consent may be required under Section 23 of the Land

Drainage Act 1991. Please see <http://www.surreycc.gov.uk/people-and-community/emergency-planning-andcommunity-safety/flooding-advice/ordinary-watercourse-consents>.

2. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
3. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damages the highway from unclean wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
 - a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
 - b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels'
 - c) Deliveries should only be received within the hours detailed in (a) above;
 - d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses included the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
 - e) There should be no burning on site;
 - f) Only minimal security lighting should be used outside the hours stated above; and
 - g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

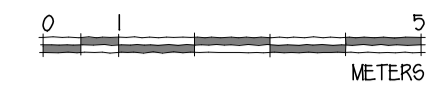
Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit. In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme (www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the essential requirements for an acceptable communication plan for surrounding properties forming part of a Method of Construction Statement are viewed as:

- a) How those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme;
 - b) How neighbours will be notified prior to any noisy/disruptive work or of any significant changes to the site activity that may affect them;
 - c) The arrangements that will be in place to ensure a reasonable telephone response during working hours;
 - d) The name and contact details of the site manager who will be able to deal with complaints; and
 - e) How those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The applicant is reminded that it is their responsibility to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant ensures that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate or combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be reached on 0800 009 3921. This is to ensure that the surface water discharge from the site shall not be detrimental to the existing sewerage system.
 7. The applicant's attention is drawn to the ACPO/Home Office Secured by Design (SBD) award scheme, details of which can be viewed at www.securedbydesign.com.
 8. The applicant is recommended to install a dropped kerb outside the bin storage and hatched lines be placed kerbside to ensure that the bins can be safely transported up and down the footpath to be emptied.
 9. The applicant's attention is drawn to the Equalities Act 2010, which requires the property to be accessible to disabled people.
 10. Please note that this application is subject to the payment of Community Infrastructure Levy (CIL). Full details of the charge, how it has been calculated and what happens next are set out in the CIL Liability Notice which will be sent separately

If you have not already done so an Assumption of Liability notice should be sent to the Council as soon as possible and before the commencement of development.

Further information on CIL and the stages which need to be followed is available on the Council's website. www.spelthorne.go.uk/CIL.



- NOTES
- 1 THE CONTRACTOR IS RESPONSIBLE FOR ALL GETTING OUT AND MUST CHECK DIMENSIONS ON SITE BEFORE WORK IS PUT IN HAND
 - 2 WRITTEN DIMENSIONS ONLY TO BE TAKEN THIS DRAWING MUST NOT BE SCALED
 - 3 THE ARCHITECT IS TO BE IMMEDIATELY NOTIFIED OF SUSPECTED OMISSIONS OR DISCREPANCIES

TREES TO BE PROTECTED & RETAINED INDICATED THIS (REFER TO APARBORICULTURE REPORT FOR T1 - T5)

NORTHERN & EASTERN BOUNDARIES TO BE FORMED FROM EXISTING BRICK WALL REDUCED TO 2m HEIGHT WITH BRICK COPING

REFER TO APARBORICULTURE REPORT FOR T1, T2 & T3 LOCATIONS HERE

DOTTED LINE INDICATES OUTLINE OF APPROVED SCHEME

SHARED AMENITY SPACE 172.3m² PLUS 2% BALCONIES & 27m² AND 4% PATIOS & 24.0m² GIVES TOTAL = 224.3m² (REQUIRED 222m²)

REAR ENTRANCE FROM PARKING & AMENITY AREA

TOTAL 14% CAR PARKING INCLUDING DESIGNATED DISABLED SPACE SURFACE FINISH BLOCK PAVER (PERMEABLE TYPE)

CONNECTION TO EX FOUL DRAINAGE RUN HERE. MH TO BE REPOSITIONED

BOUNDARIES TO STANWELL ROAD & CHAUCER ROAD TO BE 750mm HIGH METAL RAILINGS WITH HEDGING BEHIND

ALTERED VEHICULAR ACCESS FROM CHAUCER ROAD HERE

GENERAL NOTES:

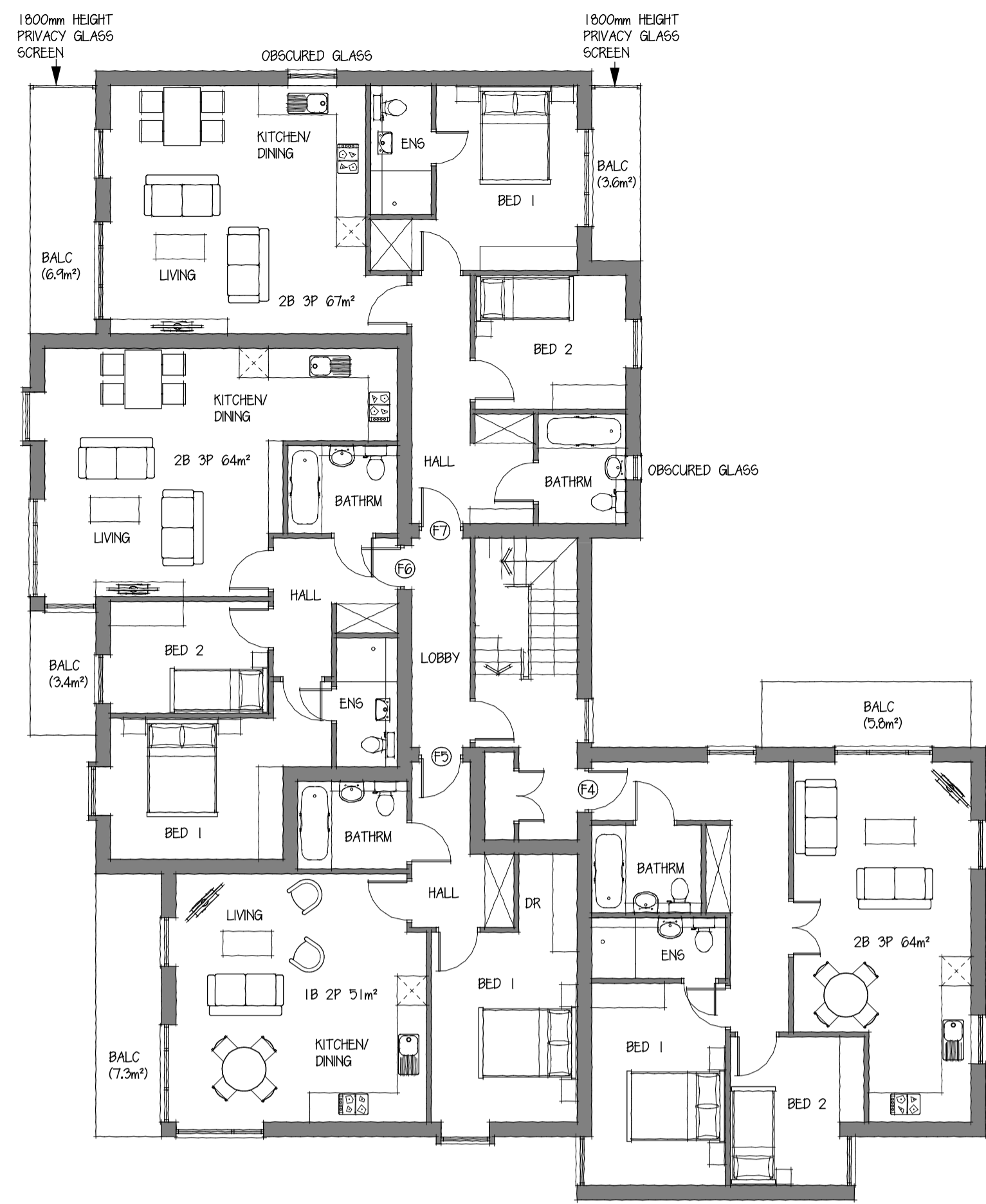
- PROPOSED 2 STOREY BUILDING TO PROVIDE 10% FLATS 2% x 1 BED AND 2% x 2 BED
- 14% CAR PARKING SPACES PROVIDED (REQUIRED 14%)
- AMENITY SPACE PROVIDED 224.3m² (REQUIRED 222m²)
- CYCLE SPACES PROVIDED 10% (REQUIRED 10%)
- BIN ENCLOSURE PROVIDED TO LA REQUIREMENTS
- COMPOST BINS, WASHING LINES & WATER BUTTS TO BE PROVIDED
- GENERAL PURPOSE (MT) BOXES TO BE PROVIDED
- ACCESS PATHS TO BE PAVED WITH FALLS TO MEET BUILDING REGULATION AD PART M

B PARKING, BINS, PATIOS & NOTES AMENDED KMD 01/09/17
 A BOUNDARY WALL NOTE AMENDED TO 2m HEIGHT KMD 10/09/17
 REVISION CHECKED DATE

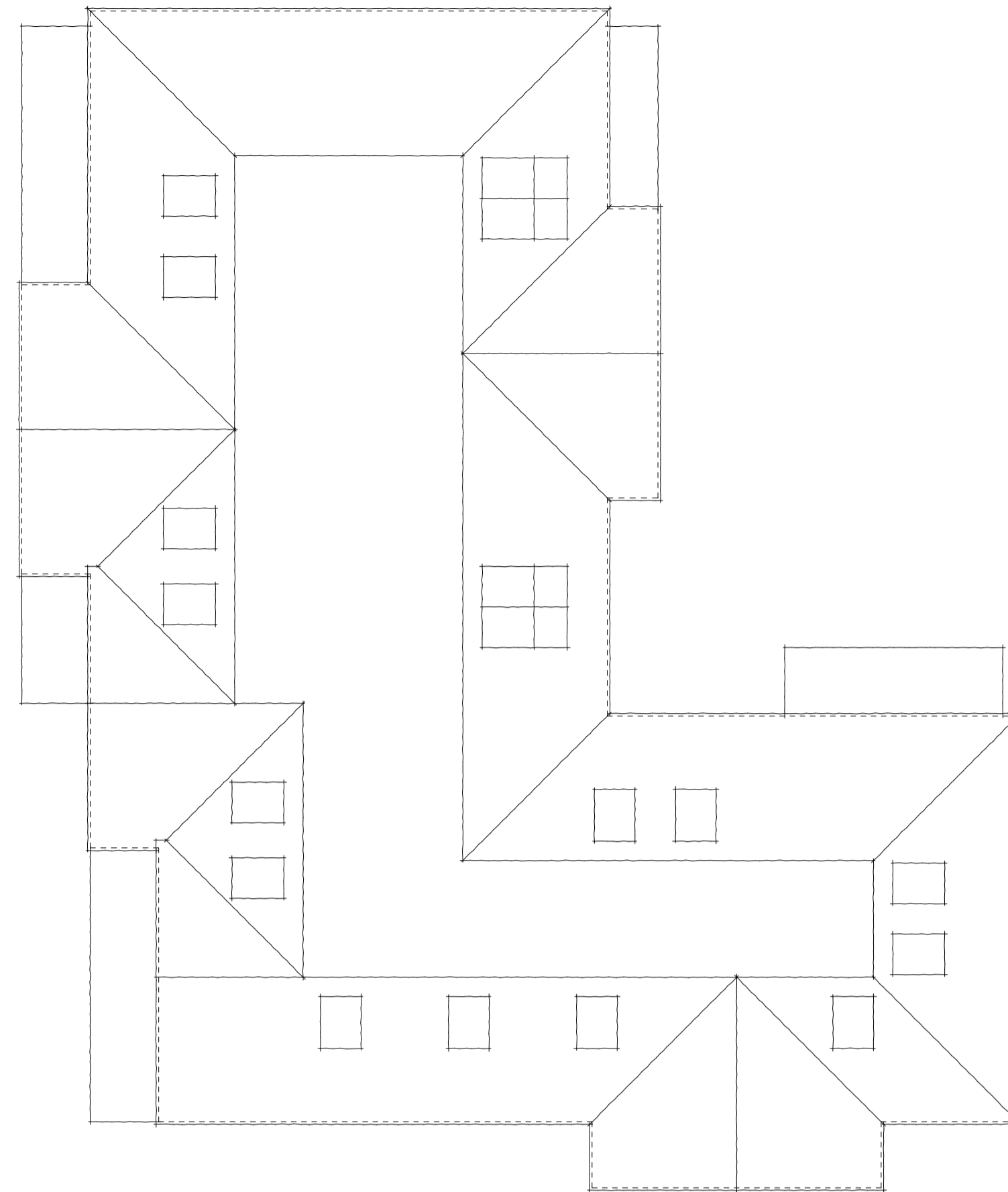
ROBERT DAVIES JOHN WEST LIMITED
 RIBA Chartered Practice
 The Courtyard 59 Church Street
 Staines upon Thames Middx TW18 4XS
 Tel: 01784 459211 E-mail: info@rdjwtd.com

DATE: 02/04/17
 SCALE: 1:100 @ A1
 DRAWN: JY
 CHECKED: KMD
 PROPOSED DEVELOPMENT, HEADLINE HOUSE ASHFORD
 PROPOSED SITE LAYOUT

DATE: 02/04/17
 SCALE: 1:100 @ A1
 DRAWN: JY
 CHECKED: KMD
 PROJECT NO: L2436/23 REV B



FIRST FLOOR PLAN

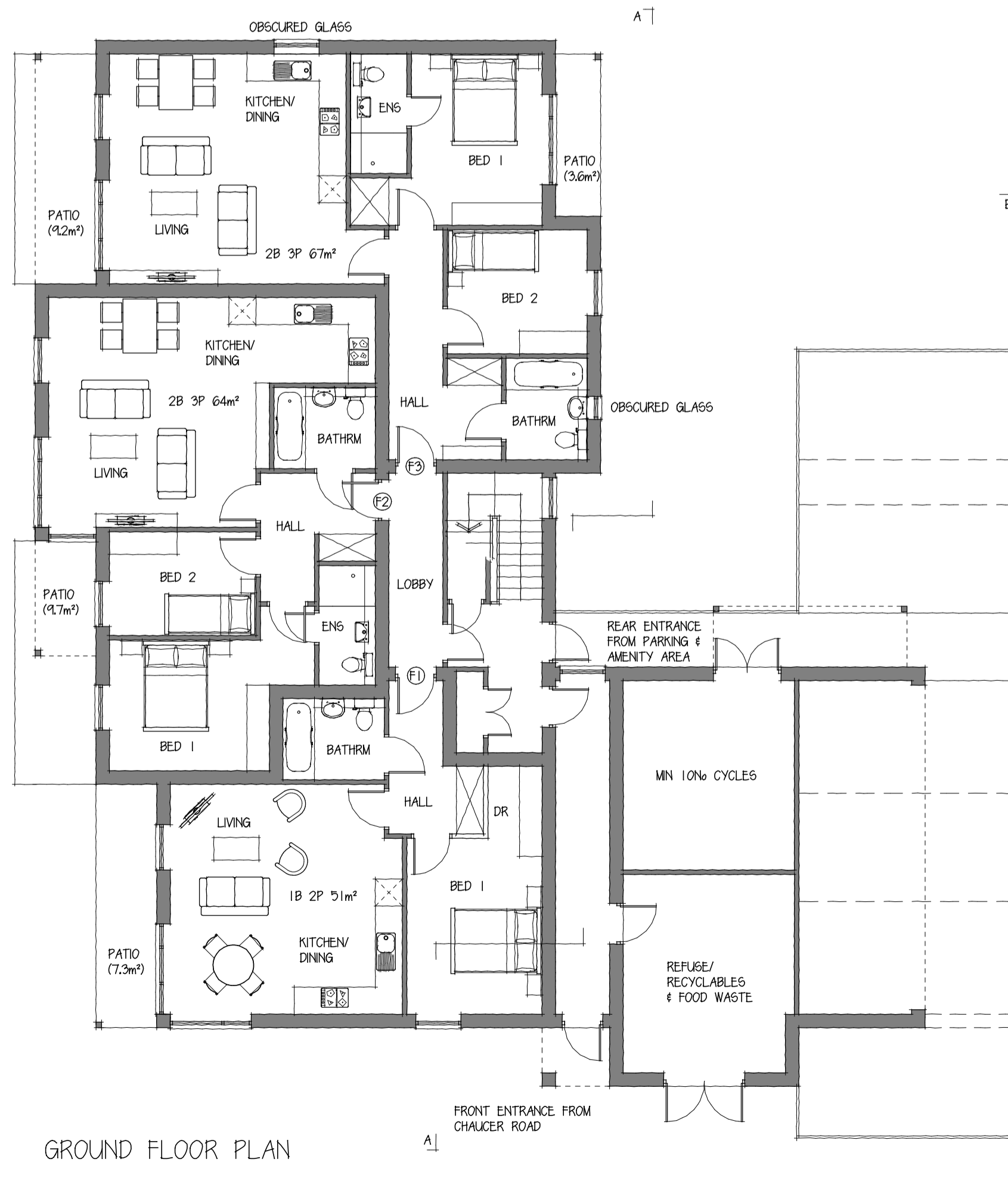


ROOF FLOOR PLAN

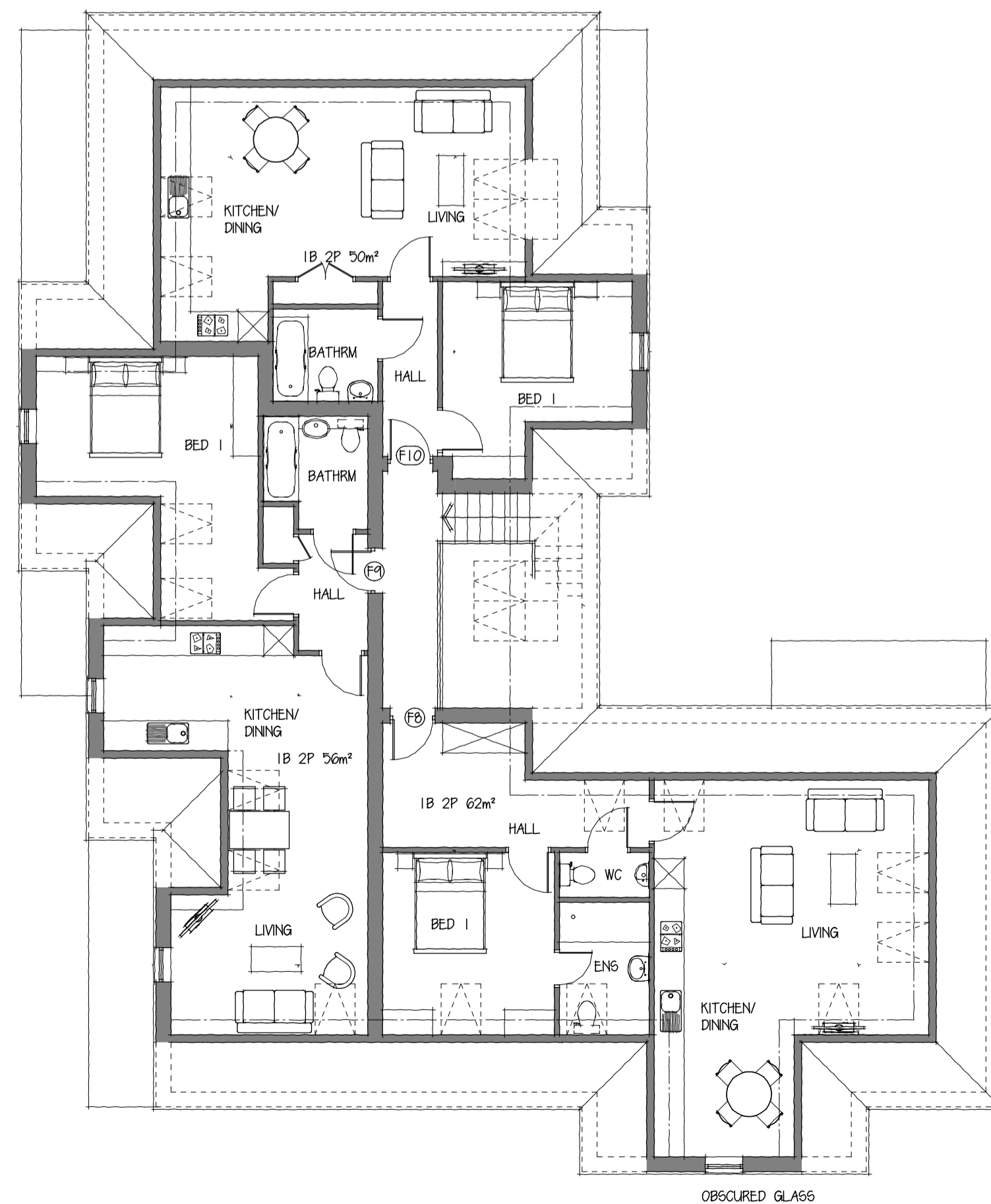
FOR EXTERNAL MATERIALS PLEASE REFER TO DRWG L2436/07 LATEST REVISION



SECTIONAL ELEVATION AA



GROUND FLOOR PLAN



SECOND FLOOR PLAN



SECTIONAL ELEVATION BB

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B PATIOS & BRG AMENDED KMD 01/09/17
 A FURNITURE & KITCHENS ADDED TO 2ND FLOOR KMD 10/05/17
 REVISION CHECKED DATE

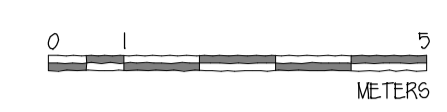
ROBERT DAVIES JOHN WEST LIMITED
 RIBA Chartered Practice
 The Courtyard 59 Church Street
 Staines upon Thames Middx TW18 4XS
 Tel: 01784 459211 E-mail: info@rdjwld.com

DATE: 28/11/16
 SCALE: 1:100 @ A1
 DRAWN: KMD
 CHECKED: AJ
 PROPOSED DEVELOPMENT,
 HEADLINE HOUSE
 ASHFORD
 PROPOSED FLOOR PLANS
 & SECTIONAL ELEVATIONS

DRWG NO: **L2436/24** REV: B



- NOTES
- 1 THE CONTRACTOR IS RESPONSIBLE FOR ALL GETTING OUT AND MUST CHECK DIMENSIONS ON SITE BEFORE WORK IS PUT IN HAND
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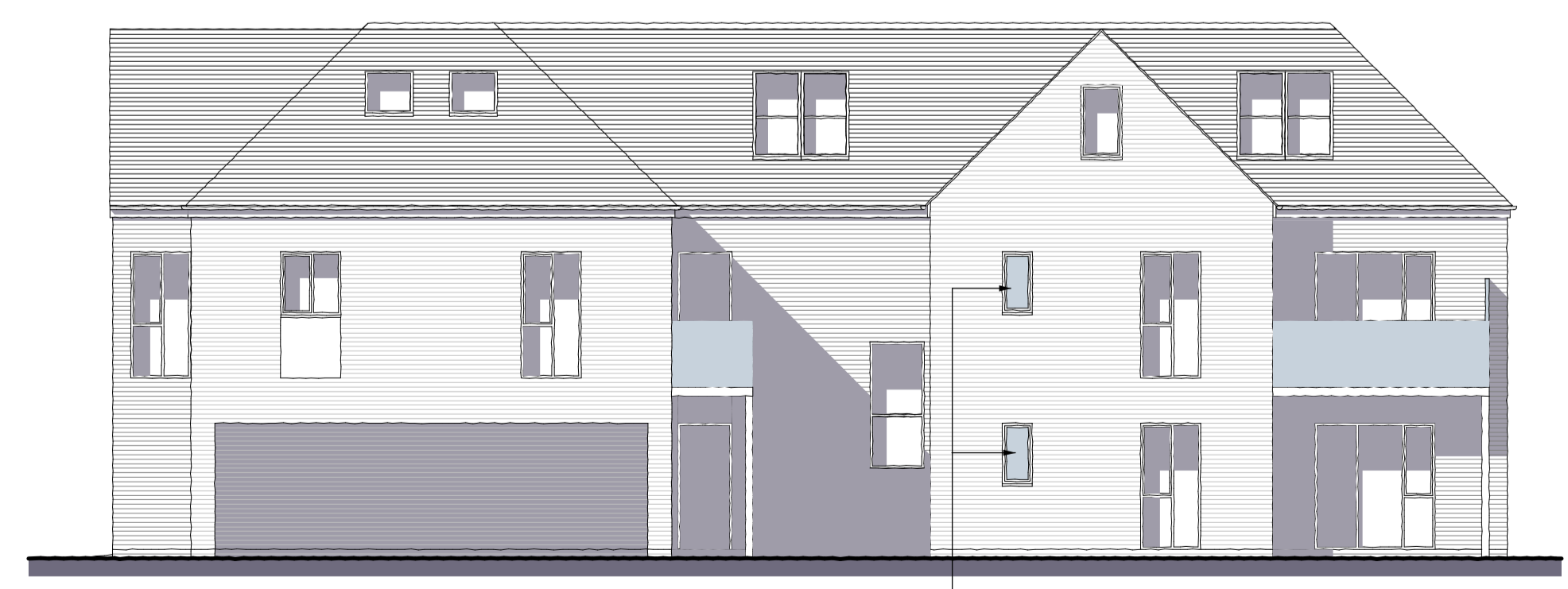


WEST ELEVATION (STANWELL ROAD)

EXTERNAL MATERIALS:
 ROOF - PITCHED 45° FINISH PLAN TILES
 PRE-FINISHED FASCIAS, SOFFITS & BARGE BOARDS, COLOUR LIGHT GREY
 DOWNPIPES & GUTTERS - GALVANISED LINDBLAD
 WALLS - BUFF & RED BRICK AS INDICATED
 WINDOWS AND DOORS - GREY POWDER COATED ALUMINIUM
 BATHROOMS & ENCLAVES TO BE OBSCURED GLAZING AS INDICATED
 GREY METAL LOUVER DOORS TO CYCLE & BIN STORES
 BALCONIES - STEEL FRAME WITH GLASS BALUSTRADES AS INDICATED
 OBSCURED GLAZING SHADED THIS:



SOUTH ELEVATION (CHAUCER ROAD)



EAST ELEVATION



NORTH ELEVATION

REVISION CHECKED DATE

ROBERT DAVIES JOHN WEST LIMITED
 RIBA Chartered Practice
 The Courtyard 59 Church Street
 Staines upon Thames MIDDLESEX TW18 4XS
 Tel: 01784 459211 E-mail: info@rdjwtd.com

DATE: 05/05/17
 SCALE: 1:100 @ A1
 DRAWN: CL
 CHECKED: KMD
 PROPOSED DEVELOPMENT,
 HEADLINE HOUSE
 ASHFORD
 PROPOSED ELEVATIONS

DWG NO: **L2436/25** REV: -